

# West Coast Wilderness Trail

- **Sub Committee**
- **Trail Overview**
- **Weather Event Impacts**
- **Projects (Enhancements)**
- **Data (Surveys & Counts)**
- **Strategy (NZCT & WCWTT)**
- **Activity Management Plan (AMP)**

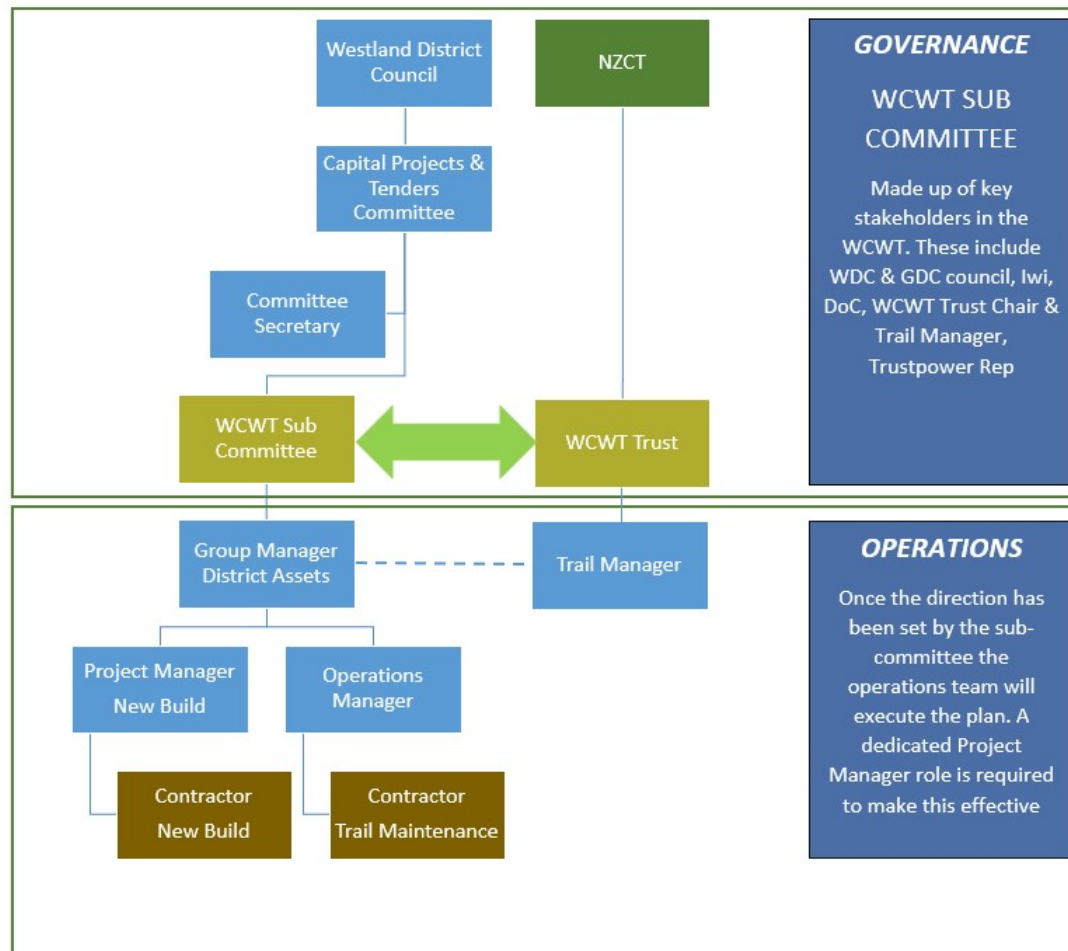
June 2020



# WCWT – Sub Committee

West Coast Wilderness Trail Governance & Reporting Structure

- MBIE / NZCT
- WDC
- Committee
- Sub Committee
- Trail Trust
- Trail Manager
- Heritage NZ



- Project Manager
- Contractors
- Land Owners
- Affected Parties
- Volunteers
- Engineers (structural)
- Archaeologist

# West Coast Wilderness Trail



Download trail map Share ride

View on Geomap

Visit the trail website



West Coast

133km / 4-5 days

ocean, rainforest & lakes / mountain views / rich heritage



**Trail Must Haves**

- Trail surface
- Gradient
- Bridges and approaches
- Signage
- Safe road crossings

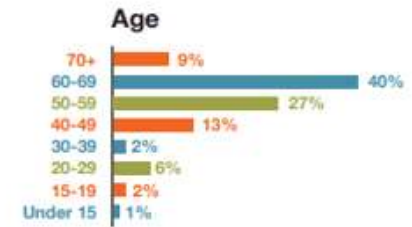
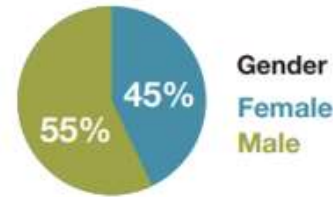
**More is Better**

- Interpretation
- Toilets
- Water / taps
- Secure parking
- Transport access

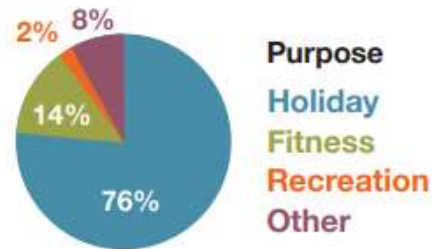
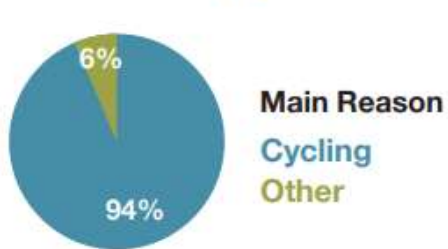
**Wow Factors**

- Viewpoints / Photostops
- Iconic track / bridge design
- Trail design and flow
- Significant heritage sites
- Artwork / Picnic areas

## 1. Rider Profile



## 2. Choosing the Wilderness Trail



**Off-Trail Must Haves**

- Safe parking
- Signage to trail start / access
- Luggage transfers / storage
- Bike repairs / parts

**More is Better**

- Food and beverage
- Accommodation
- Bike / kayak hire options
- Shuttle / bus transport
- Transport options
- Retail (in non-remote areas)

**Wow Factors**

- Iconic attractions / activities
- Cool cafes / pubs
- Unique accommodation
- All inclusive tours
- Iconic events
- A famous endorsement

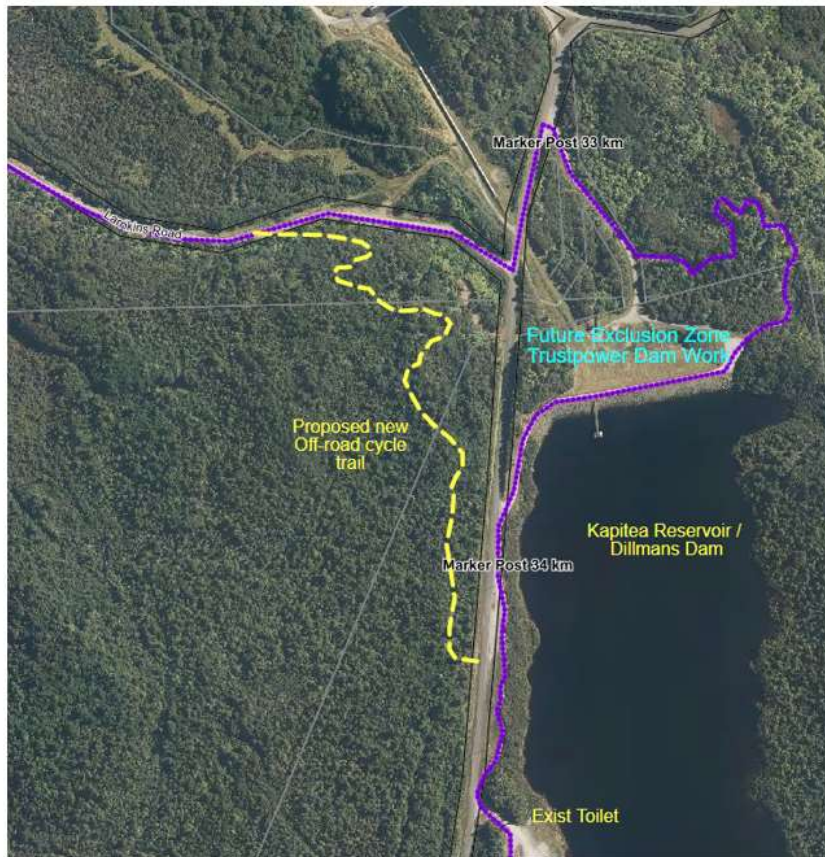
# Weather Event Impacts





# ENHANCEMENTS - FUTURE WORKS

West Coast Wilderness Trail – Larrikins Road



West Coast Wilderness Trail – Māhinapua Viewing Platform



Viewing Platform Site Proposal



Winter Views - Photo Credit: Jonathan Kennett



## Priority Project Identification Form

<b>Project Title:</b>	<b>West Coast Wilderness Trail – Kaniere Water Race Structures</b>		
<b>Project scope &amp; description:</b> Kaniere Water Race Bridge Upgrades and new 20m bridge to meet NZ Cycle Design Standard			
<b>List Project Deliverables</b>			
1. Replace Priority 1 structures and build new 20m bridge			
2. Replace Priority 2 structures and Upgrade Priority 3 structures			
<b>Project Manager:</b>	Group Manager	<b>Sponsor:</b>	Chief Executive
<b>Estimated Budget:</b>	\$ 300,000	<b>Procurement method:</b>	
<b>Construction Year :</b>	May 2021	<b>Construction Period:</b>	5 months
<b>Funding Component Breakdown:</b>		<b>Amount \$</b>	
Priority 1 Bridges replace 13m, new 20m		\$ 140,000	
Priority 2 Bridges upgrade to design standard		\$ 120,000	
Track widening at select locations		\$ 40,000	
<b>Other Resources – Staff, Facilities, External:</b>		<b>Their Role/ Details:</b>	
Trustpower and DoC input			
Designer and Structural Engineer		Specific Engineering	
<b>Key Tasks/Milestones</b>	<b>Officer Responsible</b>	<b>Expected Start Date</b>	<b>Duration</b>
Survey (completed)	Winnie Dowell-de Quadros		1 week
Design (partially completed)	Winnie Dowell-de Quadros		4 weeks
Tender and Construct			5 months
<b>Project ID:</b>			

West Coast Wilderness Trail - Kaniere Water Race Structures							
DoC # ID	WDC # ID	Length (m)	Width (m)	Structure Type	Photo	Pass/Fail	Comment / Action
	20191216104138582	5.4	0.75	Ramp		F	No action needed Alternative solution gravel trail available
031682	2019121610325785F	40.5	1.5	Boardwalk		P	Recently repaired after March 2019 storm damage
030677	2019121610235107D	10.7	1.2	Boardwalk		F (Priority 2)	Replace structure, install rails downstream & kickboard upstream
014759	201912161017499AB	3.35	1.8	Boardwalk		P	Install rail downstream, kickboard upstream
007487	201912161013420C9	7.5	0.95	Bridge		F (Priority 1)	Replace structure
043002	2019121610062776C	5	0.9	Bridge		F (Priority 1)	Replace structure
031683	20191216100339925	3.2	1.6	Bridge		P	Kickboard upstream
007490	20191216095956752	12.5	1.2	Bridge		F (Priority 2)	Replace structure
007491	2019121609524958C	14.0	1.22	Bridge		F (Priority 2)	Replace structure
031690	2019121609482725C	4.1	1.7	Bridge		P	Kickboard upstream
031685	2019121609421105E	6.0	1.7	Boardwalk		P	Kickboards
014755	2019121609261668C	19	0.85	Bridge		F (Priority 1)	New bridge required. Retain existing structure for Trustpower.

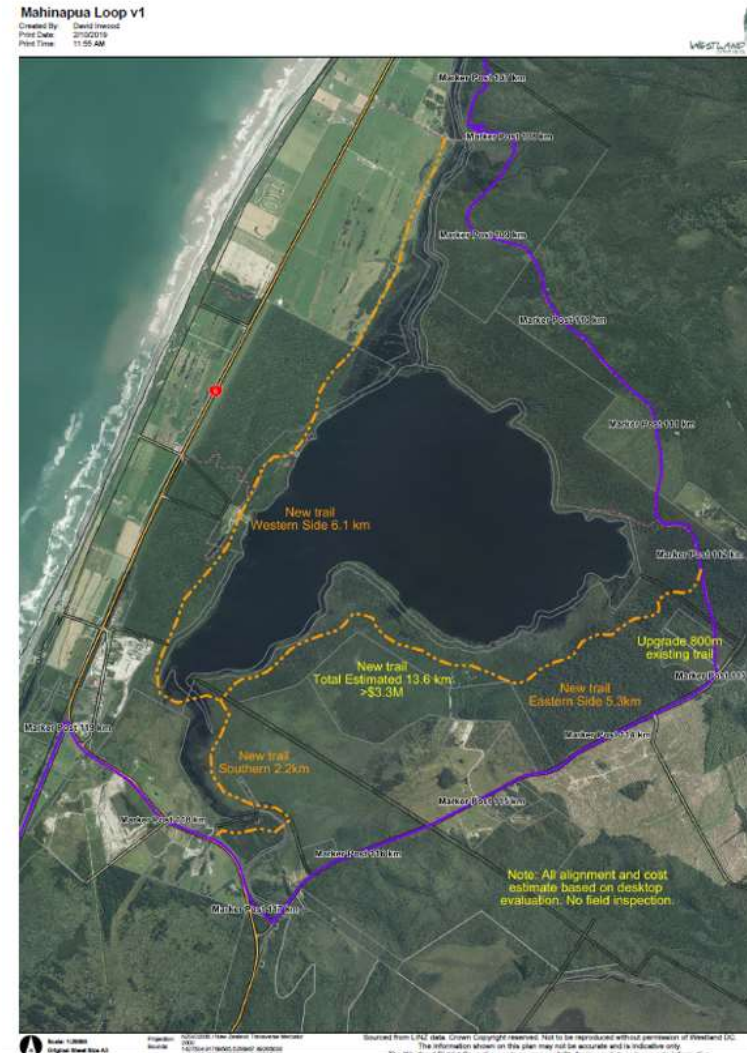




## Priority Project Identification Form

<b>Project Title:</b>	<b>West Coast Wilderness Trail – Māhinapua Lake Loop v1.2</b>		
<b>Project scope, description &amp; reason:</b>			
DoC prepared an Experience Development Plan in Sept 2012 to promote and showcase the lake and environment while increasing tourism numbers and visitor experience. This trail will complement and improve the existing Mananui-Māhinapua trail with a partnership to deliver the outcome. Increased bed night stay and visitor experience.			
<b>List Project Deliverables</b>			
<ol style="list-style-type: none"> <li>Design and approve new trail alignment with a number of land owners</li> <li>Consent and Construct new Shared trail in stages around Lake Māhinapua</li> </ol>			
<b>Project Manager:</b>	Group Manager, DoC & WDC	<b>Sponsor:</b>	Chief Executive & Director, Operations
<b>Estimated Budget:</b>	Range \$3,300,000 to \$ 4,100,000 (Staged)	<b>Procurement method:</b>	
<b>Construction Year :</b>	September 2023	<b>Construction Period:</b>	2 years
<b>Funding Component Breakdown:</b>		<b>Amount \$ Range between</b>	
Eastern Trail 5.3 km		\$1,300,00 to \$ 1,550,000	
Southern Trail 2.2 km		\$500,000 to \$ 680,000	
Western Trail 6.1 km		\$1,500,00 to \$ 1,870,000	
<b>Other Resources – Staff, Facilities, External:</b>		<b>Their Role/ Details:</b>	
Surveyor		Survey and confirm easement alignment	
Designer and Structural Engineer		Design compliant trail and structures	
<b>Key Tasks/Milestones</b>	<b>Officer Responsible</b>	<b>Expected Start Date</b>	<b>Duration</b>
Survey, Design, <u>Setout</u>			6 months
Consent & Preliminary clearing			3 months
Tender and Construct - Staged			15 months
<b>Project ID:</b>			

The preferred alignment is shown in the following diagram with somewhere in the order of 13.6km of new trail proposed. Option A - 13.6 km ± 25% = 17 km at cost of \$3.3M - \$4.1M. The trail should be staged into at least 3 construction phases.



**WCWT Lake Māhinapua, Issues & Options Summary May 2020**

Discussion document for key Stakeholders (all desktop work, no site visits, many assumptions)

OPTION	LENGTH	COST	DISCUSSION
1	6.8	\$ 1,710,000	Benefit: <ul style="list-style-type: none"> <li>• Similar to Option 2 being second shortest and cost.</li> <li>• Close proximity to lake.</li> <li>• Provides full length off-road to SH6.</li> <li>• Likely users would park at Woodstock Rd.</li> <li>• Good starting point before future full lake loop.</li> </ul> Disadvantage:
2	6.2	\$ 1,580,000	Benefit: <ul style="list-style-type: none"> <li>• Similar to Option 1 this is shortest length and cheapest cost of all options.</li> <li>• Provides full length off-road to SH6.</li> <li>• Likely users would park at Woodstock Rd.</li> <li>• Good starting point before future full lake loop.</li> </ul> Disadvantage: <ul style="list-style-type: none"> <li>• Not close proximity to lake</li> </ul>
3	9.2	\$ 2,160,000	Benefit: <ul style="list-style-type: none"> <li>• Greatest new length on eastern side without duplicating trail.</li> <li>• Provides full length off-road to SH6.</li> <li>• Close proximity to lake.</li> <li>• Includes lollypop loop to Picnic Bay.</li> <li>• Likely users would park at Woodstock Rd.</li> </ul> Disadvantage:
4	11.4	\$ 2,740,000	Benefit: <ul style="list-style-type: none"> <li>• Utilises Woodstock carpark (consider upgrading carpark &amp; installing toilet).</li> <li>• Provides full length off-road to SH6.</li> </ul> Disadvantage: <ul style="list-style-type: none"> <li>• Most expensive option taking into account all eastern side trail options.</li> <li>• Fails to include entire lake loop for this cost.</li> <li>• High structure costs crossing Frosty &amp; Maher Creeks twice.</li> </ul>
5	7.9	\$ 1,820,000	Benefit: <ul style="list-style-type: none"> <li>• Provides figure 8 loops.</li> <li>• Utilises Woodstock carpark.</li> </ul> Disadvantage: <ul style="list-style-type: none"> <li>• Fails to link with SH6, riders still on-road for 2km.</li> </ul>

OPTION	LENGTH	COST	DISCUSSION
6	7.7	\$ 1,760,000	Benefit: <ul style="list-style-type: none"> <li>• As per cheapest option 2, but extended to add strand to Mahinapua Campsite and pub.</li> <li>• Users have choice of 3 starting or end points.</li> <li>• Excellent value for money.</li> </ul> Disadvantage: <ul style="list-style-type: none"> <li>• Misses final connectivity to loop.</li> </ul>
6a	8.0	\$ 2,000,000	Benefit: <ul style="list-style-type: none"> <li>• As per option 6, but includes major bridge crossing Mirror Ck.</li> </ul> Disadvantage: <ul style="list-style-type: none"> <li>• Cost of new structure and landowner process.</li> </ul>
7	11.3	\$ 2,365,000	Benefit: <ul style="list-style-type: none"> <li>• Entire loop achieved with multiple entry or exit points.</li> </ul> Disadvantage: <ul style="list-style-type: none"> <li>• Cost of new structure and landowner process.</li> </ul>
7a	11.0	\$ 2,125,000	Benefit: <ul style="list-style-type: none"> <li>• Best cost option for full loop.</li> <li>• Comparative to Option 6.</li> </ul> Disadvantage:

Contingency of 30% should be considered for all trail options taking the range of lowest alignment cost from \$1.58 M up to \$2.05 M and highest cost option from \$2.74 M up to \$3.56 M.

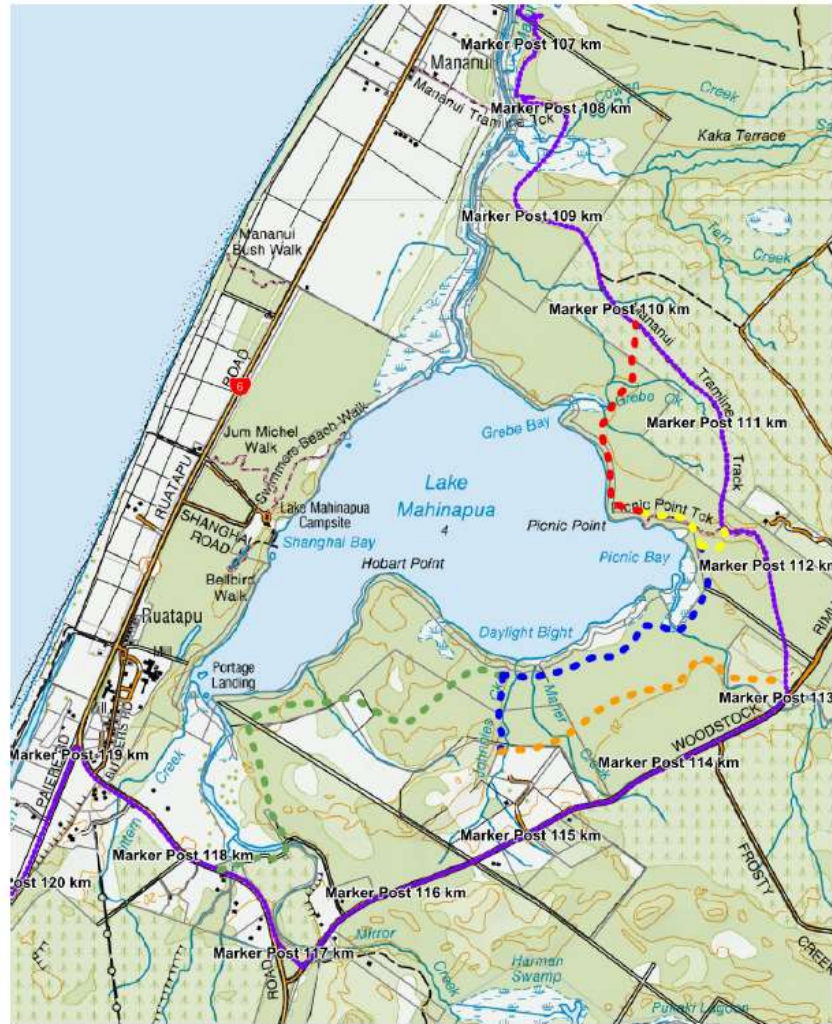
This compares with the current WDC Project Form with a cost range of \$3.3 M up to \$4.1 M which has a slightly longer overall trail loop length.

The contingency will cover engineers rough order estimate and any trail alignment changes or under-estimation of length. The costs include consenting and structural engineer input and Quality assurance on bridge construction inspections built off-site.

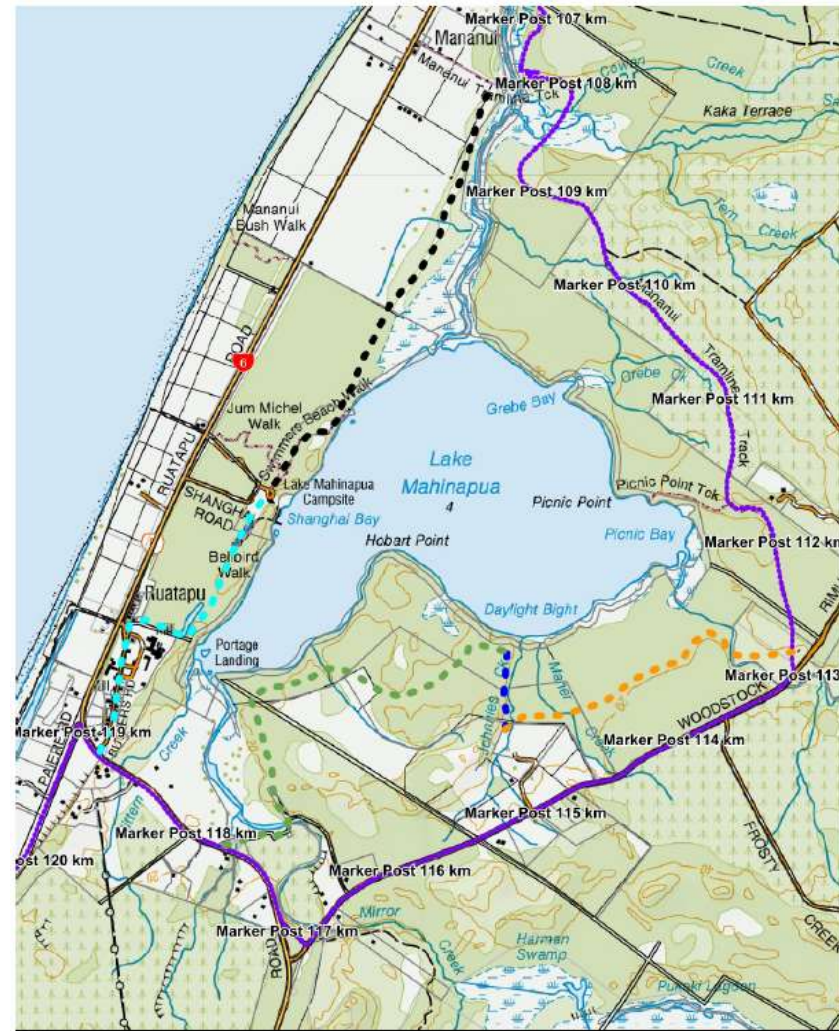
Rough order costs could be improved and likely be reduced with confirmation of easy access points to construct each section and if there is ability to build from each end rather than single strand. The new Māhinapua section is an example of a challenging and difficult-to-access site and those learnings have been applied to this overview.

The nine (9) maps show various desktop alignments to give indicative costs before more detailed work can be carried out with site inspection and following feedback from key stakeholders.

Issues & Options 4



Issues & Options 7a



2018/2019 project co-funded by MBIE MGR fund

Old Christchurch Road (includes protection of Heritage status structures)

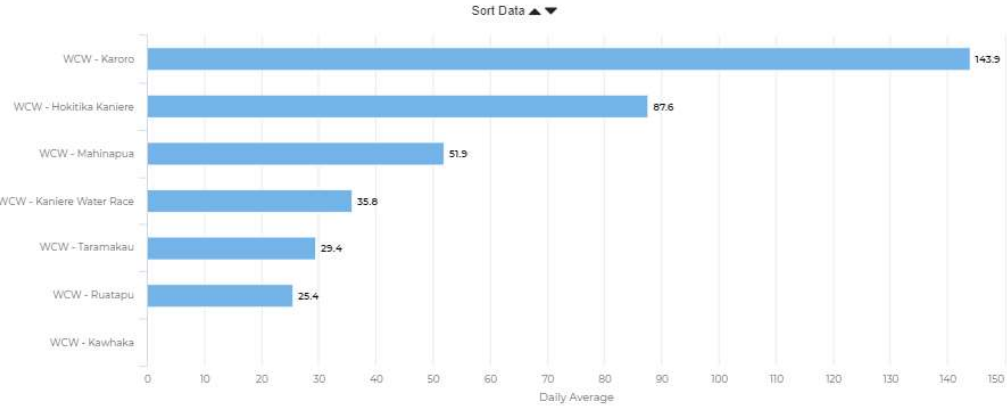


# Trail Counter Data & Survey Forms

## Key Figures Summary

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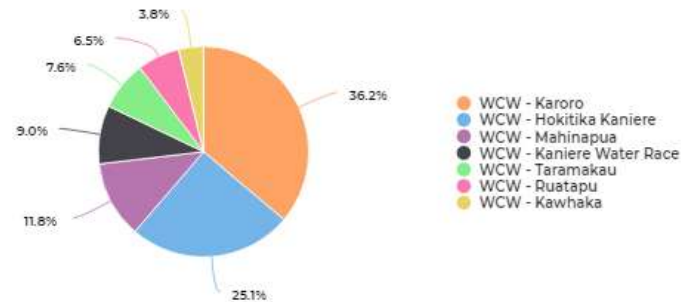
Whole Domain



## Distribution

Current Year

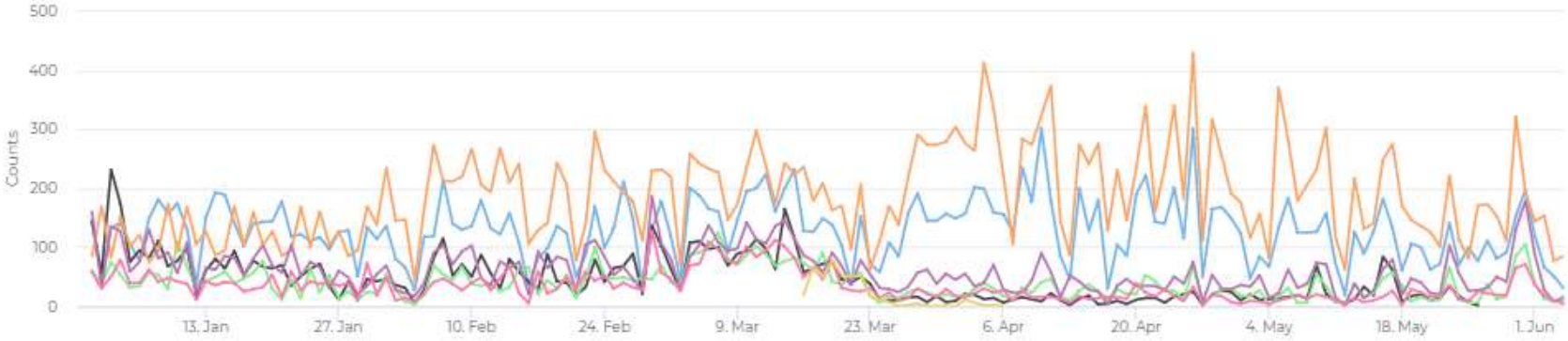
Whole Domain



## Time Series

Current Year

Whole Domain



## STRATEGIC FOCUS 1



### USER EXPERIENCE

Customer led in all our decision making based on user reporting of outstanding experiences on all Great Rides by 2021.

#### FOCUS FOR 2019/20

- Lifting performance of underperforming trails
- 16 annual WOF and Trail Performance Plans
- Customer feedback
- Experience Development Plans

#### MEASURES

- No trails less than a score of 3 (15 trails to be at 4, by December 2020)
- Experience Development plans by 30 June 2020
- 200 customer feedback surveys per trail p/a
- 10% increase on NPS for trails under 70 NPS. Trails above 70 NPS to maintain this.
- 11 Trail Inspection reports p/a



**ENABLERS** Leadership Insights – Programme to drive decision making Partnerships – to maximise initiatives and support objectives Internal – Capability and Resource

## STRATEGIC FOCUS 2



### CAPABILITY

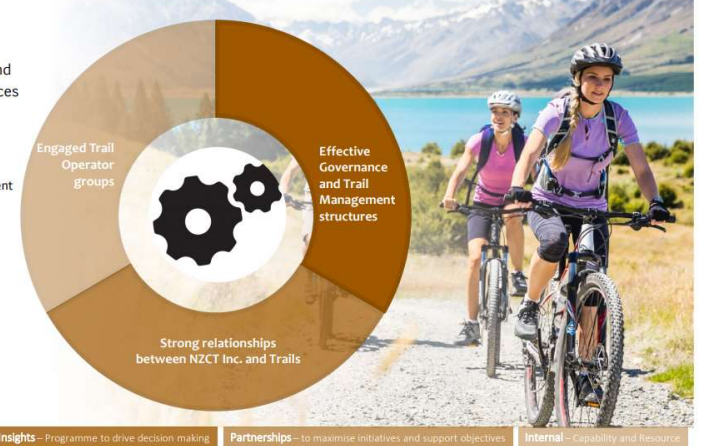
All Great Rides have governance and management capability to achieve and sustain outstanding cycling experiences by 2021.

#### FOCUS FOR 2019/20

- Effective governance and trail management structures
- All trails to have full or part time trail managers

#### MEASURES

- WOF governance score – 4, by Dec 2020
- 6 workshops with trails/operators/key stakeholders p/a
- 5 Trail meetings p/a



**ENABLERS** Leadership Insights – Programme to drive decision making Partnerships – to maximise initiatives and support objectives Internal – Capability and Resource

## STRATEGIC FOCUS 3



### SUSTAINABILITY

All Great Rides demonstrate they have the resources to achieve and maintain outstanding user experiences by 2021.

#### FOCUS FOR 2019/20

- Secure alternative funding for NZCT Inc.
- OPEX funding for Trail Managers
- Advocacy plan by Dec 2019
- Strong local government relationships

#### MEASURES

- Asset management plans by June 2020
- Financial support for Trail Managers by 30 June 2020
- Govt Funds ongoing
- Secure alternative funding for NZCT Inc. by September 2019
- Funding model for each Great Ride by December 2020



**ENABLERS** Leadership Insights – Programme to drive decision making Partnerships – to maximise initiatives and support objectives Internal – Capability and Resource

## STRATEGIC FOCUS 4



### PROMOTION

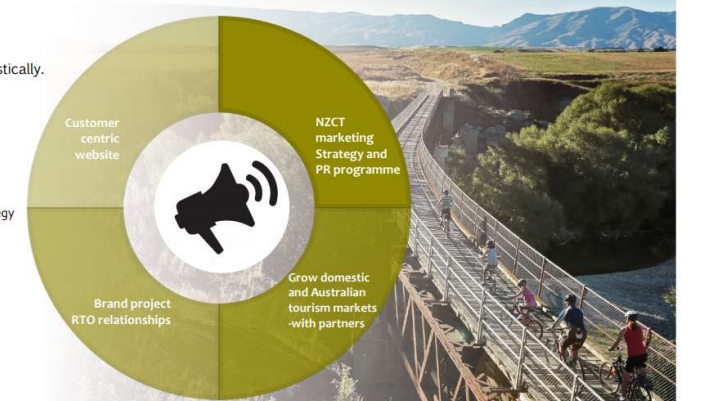
Grow awareness, consideration and desirability internationally and domestically.

#### FOCUS FOR 2019/20

- Brand project
- Website – Phase 2 mapping
- Building an online community
- Consumer enews
- Digital marketing including content strategy
- Australian marketing campaign
- PR - telling our economic impact stories

#### MEASURES

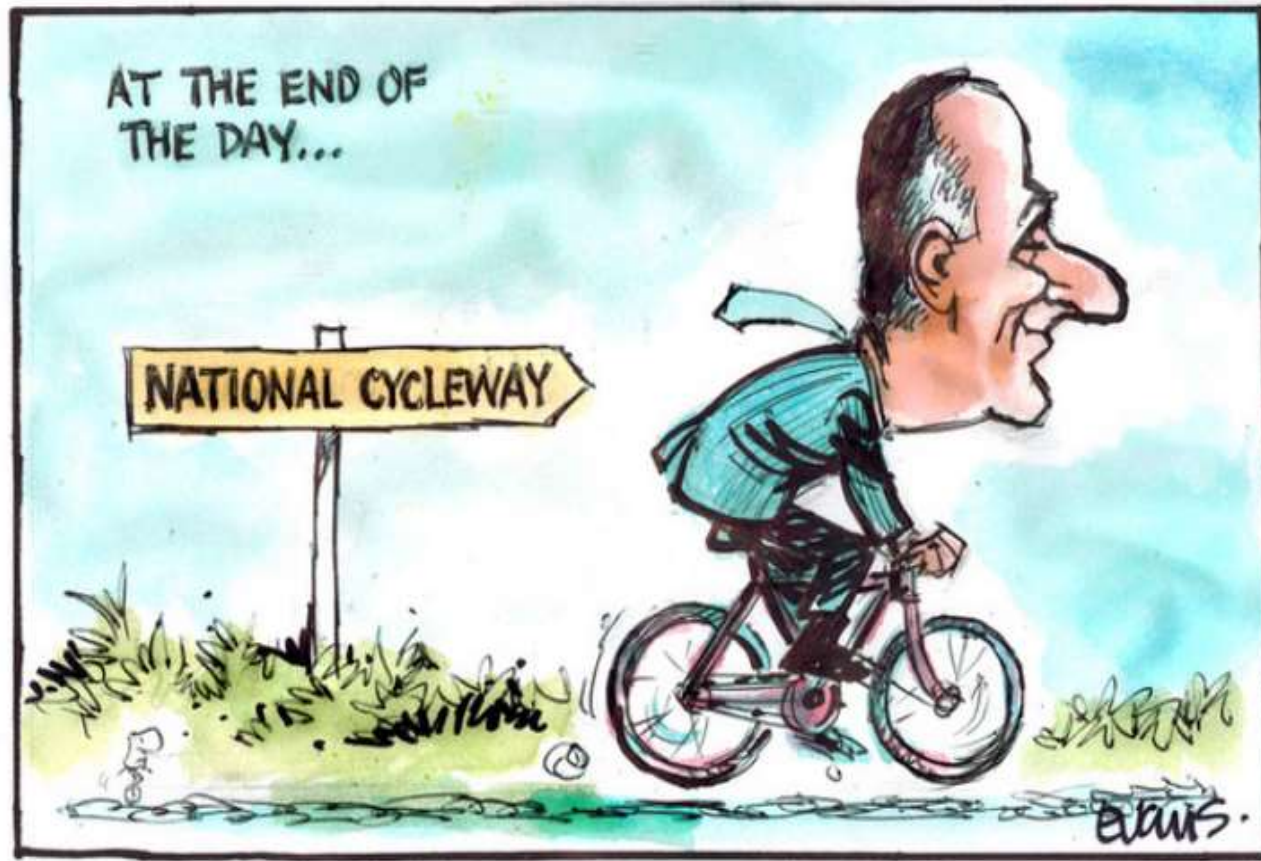
- 10% increase of trail users p/a
- 10% increase in Australian cyclists p/a
- Database of 10,000 by June 2020
- Website: Unique users, repeat users and time on site



**ENABLERS** Leadership Insights – Programme to drive decision making Partnerships – to maximise initiatives and support objectives Internal – Capability and Resource

WEST COAST  
WILDERNESS TRAIL

NGA HAERENGA  
THE NEW ZEALAND CYCLE TRAIL



# Activity Management Plan



*West Coast Wilderness Trail Activity Management Plan*

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